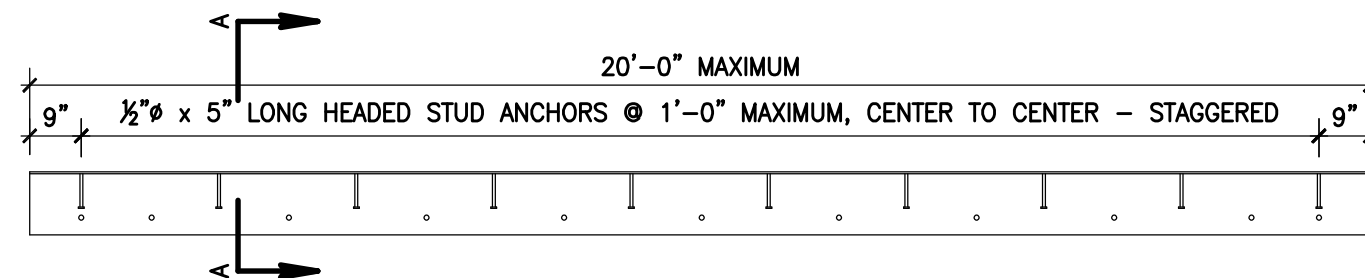


DETAIL SECT A-A

SCALE 3"=1'-0"

TYPICAL SECTION

SCALE 3"=1'-0"



ELEVATION - STEEL FACING

SCALE 3/8"=1'-0"

NOTES

1. EXPANSION JOINTS IN THE STEEL CURB FACING AND CONCRETE BACKING SHALL BE AT A MAXIMUM SPACING OF 24 FEET.
2. THE EXPANSION JOINTS OF THE CURB AND STEEL CURB FACING SHALL LINE UP WITH THE EXPANSION JOINTS OF THE CONCRETE SIDEWALKS, WHEREVER POSSIBLE.
3. NO PIECE OF STEEL CURB FACING HAVING LESS THAN TWO (2) WELDED DOWELS MAY BE INSTALLED UNLESS IT IS WELDED TO THE ADJACENT STEEL CURB FACING.
4. 1/2"φ x 5" HEADED ANCHOR STUDS (GRANULAR OR SOLID FLUX FILLED) MAY BE SUBSTITUTED.
5. STRUCTURAL STEEL (A.S.T.M. DESIGNATION A36)
6. SURFACE SHALL BE THOROUGHLY CLEANED AND THEN PAINTED IN ACCORDANCE WITH D.O.T. SPECIFICATIONS, SECTION 2.13. LATEST REVISION. COLOR OF TOP COAT SHALL BE GRAY AS APPROVED BY THE ENGINEER.
7. WHERE TWO (2) PIECES OF STEEL CURB FACING ARE JOINED BUT NOT WELDED, TWO (2) ONE-HALF (1/2) INCH RODS, TWENTY-FOUR (24) INCHES LONG SHALL BE INSERTED INTO THE CONCRETE BACKING, ONE-HALF (1/2) THE LENGTH AT EACH SIDE OF THE JOINT
8. CONCRETE TO BE CLASS B-32, AIR ENTRAINED.
9. CORNER CURB: - VERTICAL FACE WILL BE ACCEPTABLE FOR CORNER CURBS PROVIDING THE ENDS ARE WARPED TO FORM A TRANSITION WITH ADJACENT BATTERED FACE CURBS

BENDALL NOTES

1. NOTES 1-9 ARE COPIED EXACTLY AS THEY APPEAR ON THE CITY OF NEW YORK DWG, H-1010 REVISED 7-1-2010
2. THE DRAWING SHOWN IS A REPLICA OF THE DRAWING ON NYC DWG H-1010 EXCEPT THE ANCHOR STUDS ARE SHOWN HEADED, ON DRAWING H-1010 THEY ARE SHOWN BENT.
3. THE SURFACE WILL BE CLEANED AND SHALL RECEIVE A SHOP APPLIED PRIME COAT AND A SHOP APPLIED INTERMEDIATE COAT OF EPOXY MASTIC ALUMINUM MANUFACTURED BY SHERWIN WILLIAMS OR AN APPROVED EQUAL. AS LISTED IN THE NYSOT MATERIALS AND EQUIPMENT APPROVED LIST FOR STRUCTURAL STEEL PAINT, CLASS 2.
4. IN ACCORDANCE WITH NYC DOT SPECIFICATIONS A URETHANE TOP COAT THAT IS ALSO MANUFACTURED BY SHERWIN WILLIAMS OR AN APPROVED EQUAL SHALL BE APPLIED IN THE FIELD BY OTHERS..

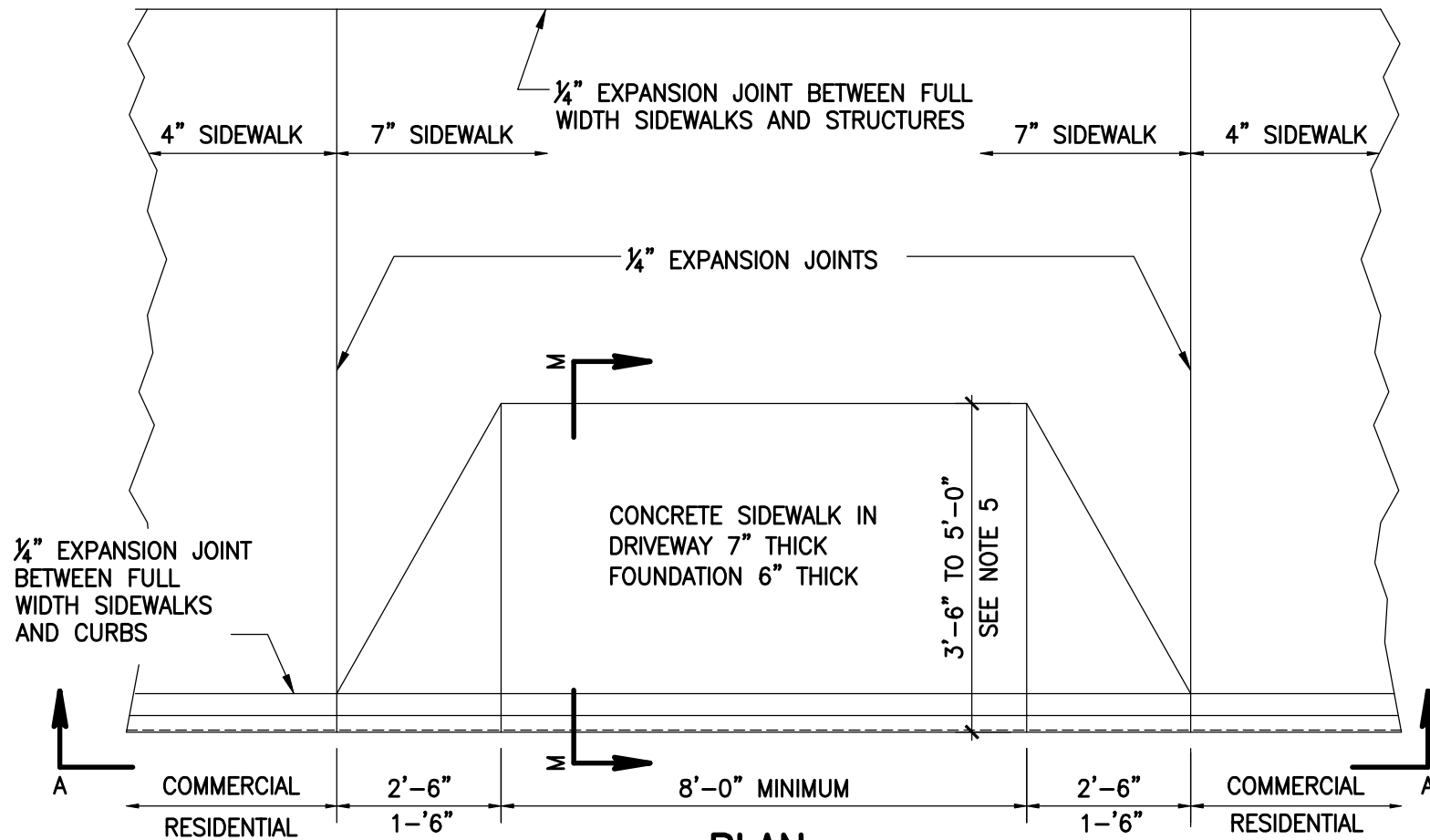
BENDALL INDUSTRIES, INC. 
 4459 BRONX BLVD
 BRONX, NY 10470
 718-325-1588V 718-325-6685F

SCALE: 3"-1'-0"
 BY: LW
 DATE: 1-21-2013

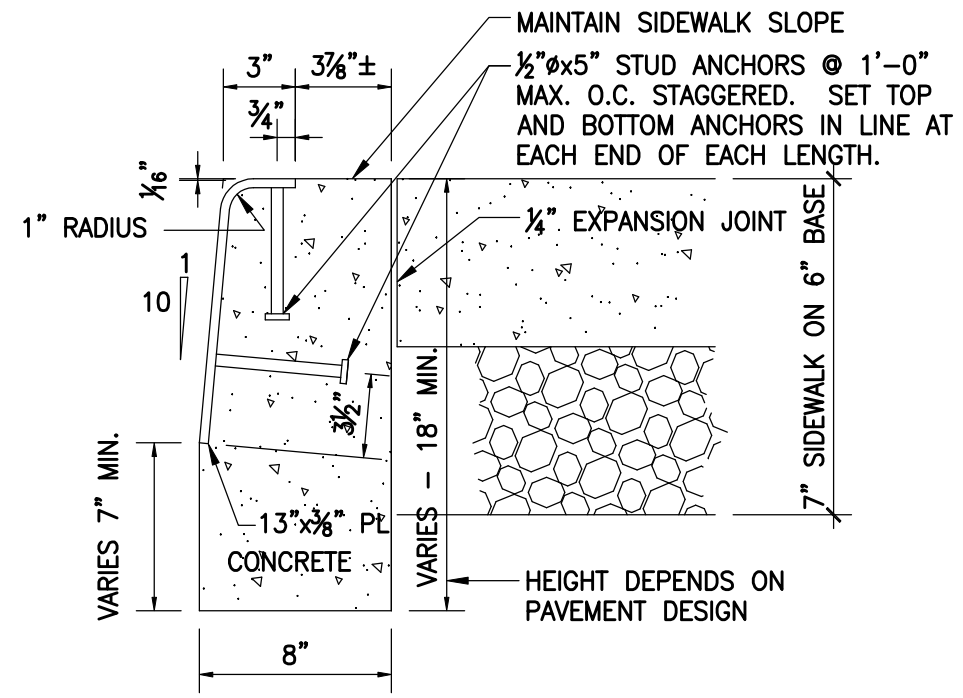
STEEL FACED CURB
 STEEL FACING TYPE D
 REF. NYC DOT DWG H-1010 7-1-2010

DRAWING
 0818-22
 REV-3



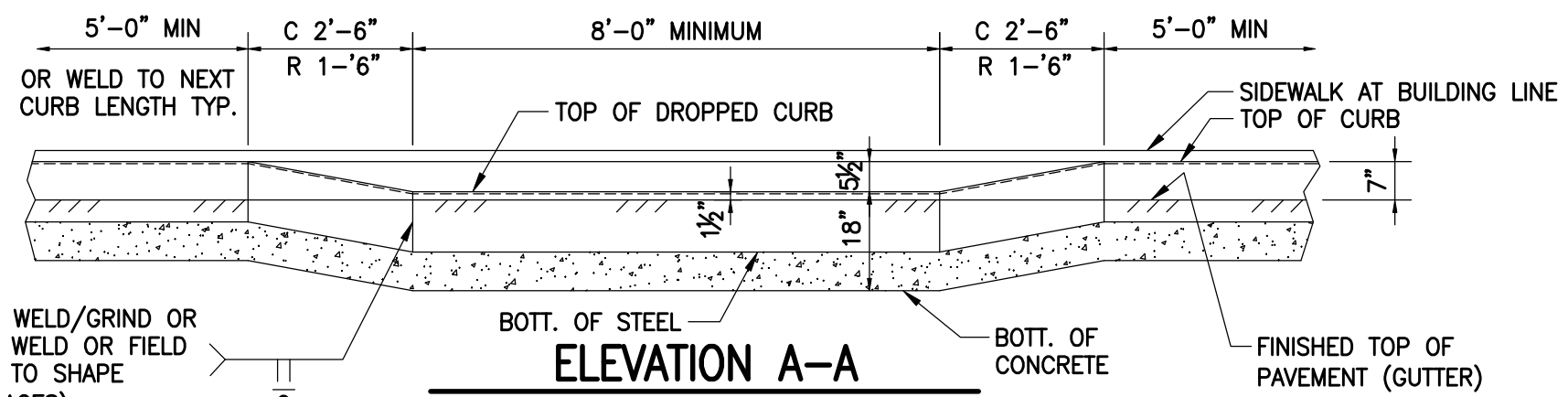


PLAN
SCALE $\frac{3}{8}'' = 1'-0''$



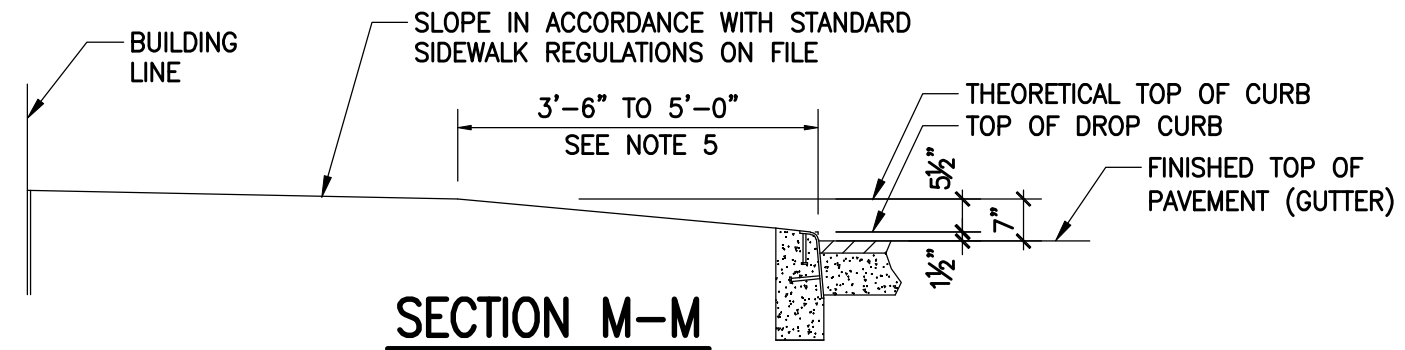
DETAIL - STEEL FACED CURB
SCALE $\frac{1}{2}'' = 1'-0''$

- NOTES**
- $\frac{1}{2}'' \times 5''$ HEADED ANCHOR STUDS (GRANULAR OR SOLID FLUX FILLED) MAY BE SUBSTITUTED
 - STRUCTURAL STEEL AS PER BOARD OF STD SPECS. 20-0-38 TYPE A-1 (ASTM A36)
 - STEEL FACING TO BE CLEANED AND PAINTED ACCORDING TO SUBSECTION 2.13.4 OF THE NYCDOT STANDARD HIGHWAY SPECIFICATION. THE COLOR OF THE TOP COAT SHALL BE GRAY AS APPROVED BY THE ENGINEER.
 - CONCRETE TO BE CLASS B-32, TYPE 11A.
 - 3'-6" TO 5'-0" AS ORDERED BY ENGINEER EXCEPT FOR THE FIRE DEPARTMENT DRIVEWAYS WHICH WILL SLOPE STRAIGHT BACK TO THE PROPERTY LINE, FIRE DEPARTMENT DRIVEWAYS SHALL BE TYPE III SIDEWALK - SEE SHT. H-1045



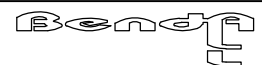
ELEVATION A-A
(DRAWN FOR 7" REVEAL AT ADJACENT CURBS)
SCALE $\frac{3}{8}'' = 1'-0''$

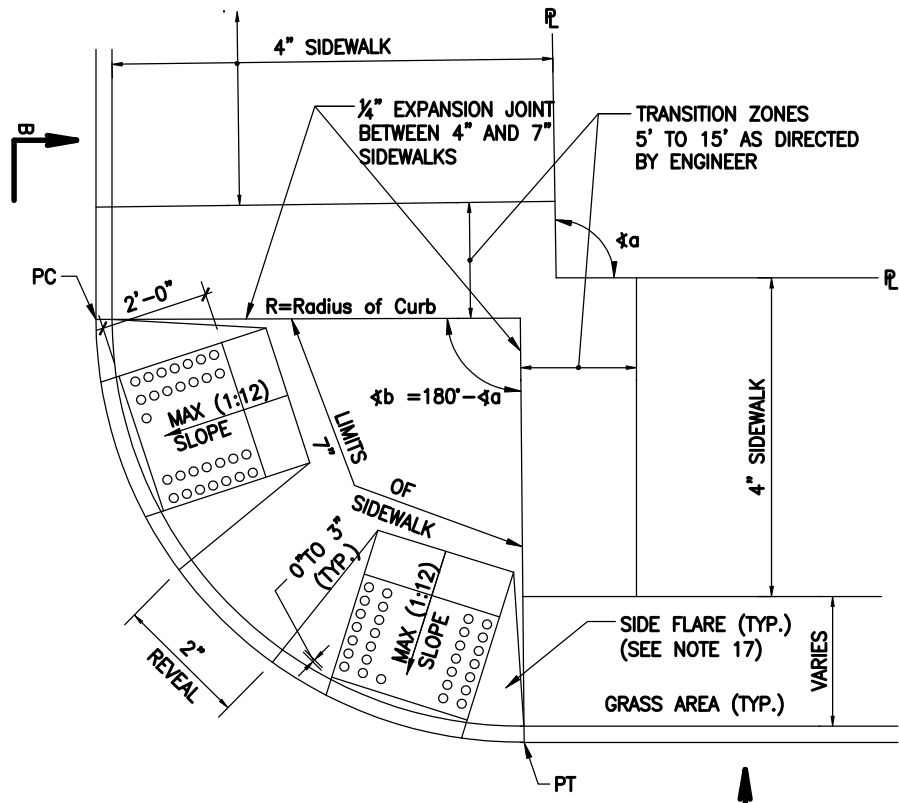
- BENDALL NOTES**
- NOTES 1-5 ARE COPIED EXACTLY AS THEY APPEAR ON THE CITY OF NEW YORK DWG, H-1015 REVISED 7-1-2010
 - THE DRAWING SHOWN IS A REPLICA OF THE DRAWING ON NYC DWG H-1015 EXCEPT THE ANCHOR STUDS ARE SHOWN HEADED, ON DRAWING H-1015 THEY ARE SHOWN BENT. ALSO THERE IS MORE INFORMATION THAN IS SHOWN ON DWG H-1015 AND ALL THE VIEWS ARE TO SCALE



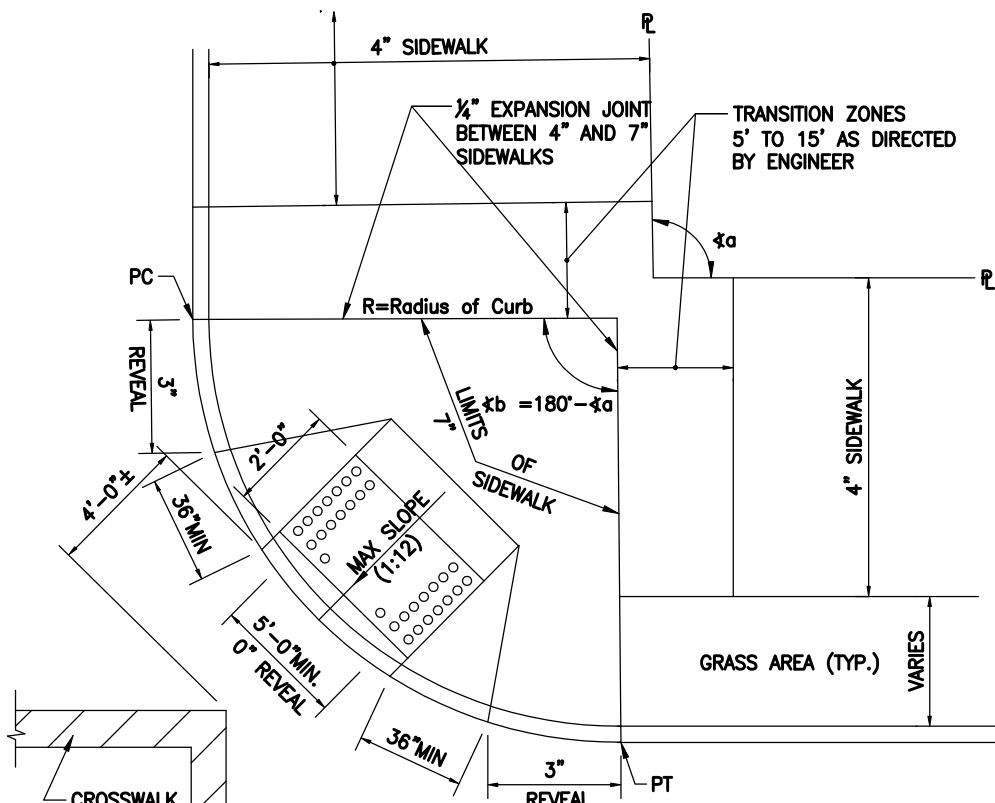
SECTION M-M
 $\frac{3}{8}'' = 1'-0''$



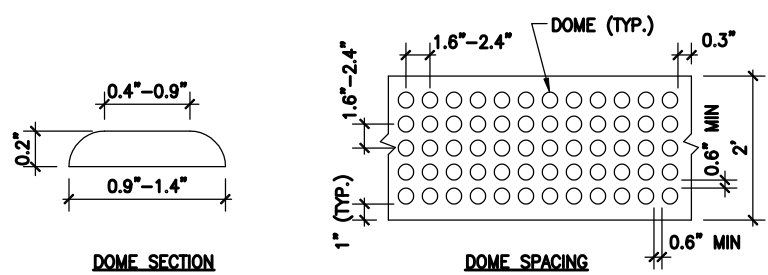
BENDALL INDUSTRIES, INC.  4459 BRONX BLVD BRONX, NY 10470 718-325-1588V 718-325-6685F	SCALE: 3"-1'-0" BY: LW DATE: 1-21-2013
	STEEL FACED CURB STEEL FACING TYPE D REF. NYC DOT DWG H-1015 7-1-2010



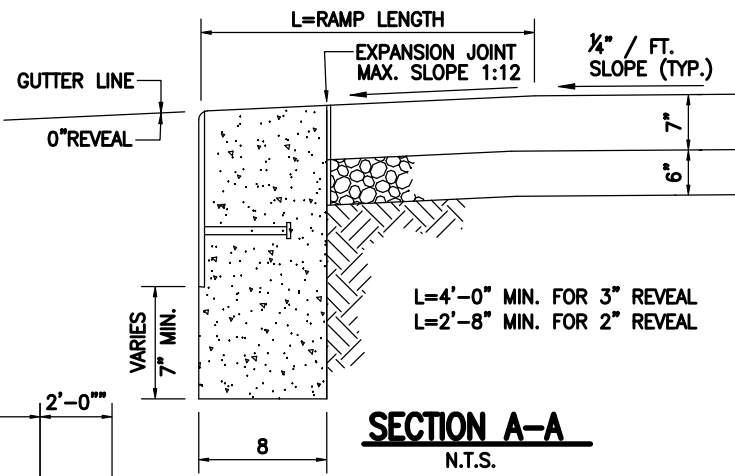
PLAN - CASE I, CASE III
N.T.S.



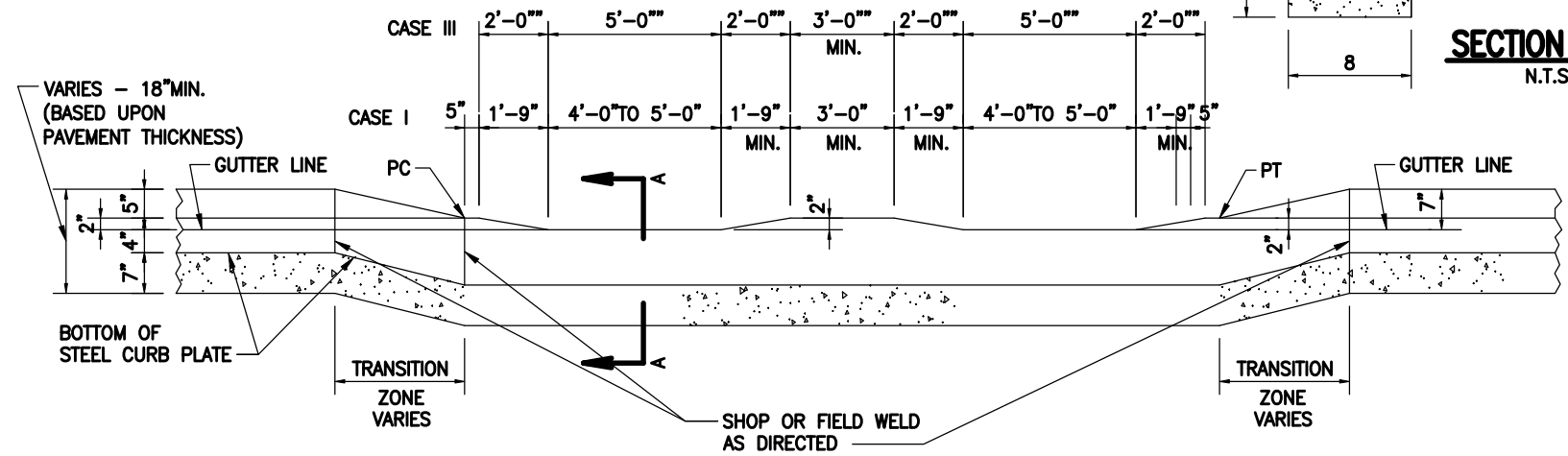
PLAN - CASE II
N.T.S.



DETECTABLE WARNING SURFACE DETAILS



SECTION A-A
N.T.S.



ELEVATION B-B
N.T.S.

NOTES

- REFER TO H-1010 (LATEST REVISION) FOR STEEL FACED CURB-TYPE D.
- ALL MATERIALS AND CONSTRUCTION METHODS USED SHALL CONFORM TO SECTIONS #4.08 / 4.09 / 4.13 / 4.13DE OF THE STANDARD SPECIFICATIONS, LATEST EDITION, AS AMENDED.
- WHEN INSTALLING PEDESTRIAN RAMPS IN OTHER THAN PRE ENGINEERED CAPITAL RECONSTRUCTION PROJECTS, ALLOWANCE SHALL BE MADE FOR EXISTING CONDITIONS PROVIDED THAT THE SLOPE OF THE RAMP SHALL NOT EXCEED 1:12 AND THE ZERO INCH REVEAL IS OBTAINED. TO INSURE THAT SOUND ENGINEERING JUDGEMENT IS USED IN MEETING EXISTING CONDITIONS. ANY AND ALL VARIATIONS FROM THE DETAILS OF CONSTRUCTION HEREIN SHOWN MUST HAVE THE APPROVAL OF BOTH THE ASSISTANT COMMISSIONER OF DESIGN AND THE ASSISTANT COMMISSIONER OF CONSTRUCTION.
- CASE II PLAN SHALL BE USED ONLY WHERE EXPLICITLY DIRECTED BY THE ENGINEER AND APPROVED BY THE COMMISSIONER PRIOR TO DESIGN/INSTALLATION.
- SURFACE OF ALL PEDESTRIAN RAMPS SHALL BE STABLE, FIRM AND SLIP RESISTANT. CONCRETE RAMP SURFACE SHALL HAVE A COARSE BROOM FINISH RUNNING PERPENDICULAR TO THE SLOPE, EXCLUSIVE OF THE DETECTABLE WARNING FIELDS.
- LANDINGS BETWEEN THE PROPERTY LINES AND THE BACK EDGE OF RAMPS SHALL HAVE A MINIMUM CLEAR DIMENSION OF 5 FT. BY 5 FT. SQUARE; HOWEVER, WHERE CASE I RAMPS ARE USED THE MINIMUM CLEAR DIMENSION SHALL BE 4 FT. BY 4 FT. SQUARE. THE MAXIMUM CROSS SLOPE AT LANDINGS IS 1/4" PER FOOT IN ANY DIRECTION. LANDINGS MAY OVERLAP WITH ADJACENT LANDINGS OR A SINGLE LANDING MAY SERVE MULTIPLE CURB RAMPS.
- ALL EXPOSED STEEL SURFACES SHALL BE GROUND SMOOTH.
- ON FULL WIDTH SIDEWALKS, EXPANSION JOINTS TO BE PLACED AT BUILDING FACES, STRUCTURES AS WELL AS AT BACK FACE OF CURB.
- ALL DIMENSIONS AND NOTES SHALL BE APPLICABLE TO GRANITE CURB INSTALLATIONS AND / OR CONCRETE CURB INSTALLATIONS.
- THE FOLLOWING GUIDELINES SHALL BE APPLIED IN DETERMINING THE APPLICATIONS OF THE SPECIFIC CASES WHERE THE INTERIOR $\phi b = 180 - \phi a$

CASE I FOR CORNERS WITH	CASE II FOR CORNERS WITH	CASE III FOR CORNERS WITH
R=12' INTERIOR $\phi b \geq 90^\circ$	R<12' INTERIOR ϕb ANY ANGLE	R>15' INTERIOR ϕb ANY ANGLE
R=13' INTERIOR ϕb BET. 83' & 93'	R=12' INTERIOR $\phi b < 90^\circ$	R=15' INTERIOR $\phi b \geq 81^\circ$
R=14' INTERIOR ϕb BET. 77' & 90'	R=13' INTERIOR $\phi b < 83^\circ$	R=14' INTERIOR $\phi b \geq 90^\circ$
R=15' INTERIOR ϕb BET. 72' & 81'	R=14' INTERIOR $\phi b < 77^\circ$	R=13' INTERIOR $\phi b \geq 93^\circ$
	R=15' INTERIOR $\phi b < 72^\circ$	
- THE DETAILS PROVIDED ARE NOT DRAWN TO SCALE, THE QUANTITY OF DOMES DEPICTED ON THE DETECTABLE WARNING FIELD (THE DOMES AND THE ENTIRE 2 FT. WIDE SURFACE) IS FOR ILLUSTRATION ONLY.
- THE SIZE OF THE DETECTABLE WARNING FIELD SHALL BE 2 FT. IN THE DIRECTION OF TRAVEL AND SHALL EXTEND THE FULL WIDTH OF THE CURB RAMP AS SHOWN, EXCLUSIVE OF SIDE FLARES.
- DETECTABLE WARNINGS SHALL BE LOCATED SO THAT THE EDGE OF THE WARNING FIELD NEAREST TO THE ROADWAY OR STREET SURFACE IS 0" TO 3" FROM THE BACK OF THE CURB, AS SHOWN.
- DOMES ALIGNMENT, DOMES SHALL BE ALIGNED ON A SQUARE GRID IN THE PREDOMINANT DIRECTION OF TRAVEL.
- COLOR REQUIREMENT. THE DETECTABLE WARNING FIELD SHALL BE THE COLOR SPECIFIED IN THE CONTRACT DOCUMENTS OR SHALL VISUALLY CONTRAST WITH THE ADJOINING CURB RAMP, OR OTHER ADJACENT WALKWAY SURFACES WHERE THERE IS NO CURB RAMP, EITHER LIGHT-ON-DARK OR DARK-ON-LIGHT AS DEFINED IN THE AMERICANS WITH DISABILITIES ACCESSIBILITY GUIDELINES (ADAAG).
- PAYMENT LINES FOR DETECTABLE WARNING UNITS ARE THE 2 FT. DIMENSION SHOWN IN THE DETAILS EXTENDING THE FULL WIDTH OF THE CURB RAMP.
- WHEN STREET FURNITURE (LAMP POSTS, TRAFFIC SIGNAL POSTS, UTILITY POSTS, HYDRANTS, ETC.) INTERFERES WITH THE CONSTRUCTION OF A SIDE FLARE ADJACENT TO A NON-WALKING (GRASS) AREA, SAID SIDE FLARE SHALL BE REPLACED WITH A CONCRETE CURB, AS DIRECTED BY THE ENGINEER, PROVIDED THE INTERFERENCE CAN BE AVOIDED AND LEFT IN PLACE.
- IN CASE OF INFEASIBILITY AND IN ORDER TO CLEAR INTERFERENCE WITH THE STREET FURNITURE (LAMPPOSTS, TRAFFIC SIGNAL POSTS, UTILITY POSTS, HYDRANTS, ETC.) UPON DOCUMENTATION AND APPROVAL BY THE ASSISTANT COMMISSIONER OF THE CONSTRUCTION DIVISION, THE WIDTH OF THE RAMP COULD BE REDUCED TO NO LESS THAN 3'-0" FEET.
- PEDESTRIAN RAMPS SHALL BE INSTALLED PARALLEL TO THE CROSSWALKS WHERE FEASIBLE. HOWEVER, WHERE DUE TO OBSTRUCTIONS AND INTERFERENCE WITH THE STREET FURNITURE (LAMPPOSTS, TRAFFIC SIGNAL POSTS, UTILITY POSTS, HYDRANTS, ETC.) IT WOULD NOT BE FEASIBLE TO INSTALL THE RAMPS PARALLEL TO THE CROSSWALKS, UPON DOCUMENTATION AND AS ORDERED AND APPROVED BY THE ENGINEER THE RAMPS COULD BE REORIENTED TO CLEAR THE OBSTRUCTIONS.

BENDALL NOTES

- NOTES 1-19 ARE COPIED EXACTLY AS THEY APPEAR ON THE CITY OF NEW YORK DWG, H-1011 REVISED 7-1-2010
- THE DRAWING SHOWN IS A REPLICA OF THE DRAWING ON NYC DWG H-1011 EXCEPT THE ANCHOR STUDS ARE SHOWN HEADED. ON DRAWING H-1011 THEY ARE SHOWN BENT.
- THE SURFACE WILL BE CLEANED AND SHALL RECEIVE A SHOP APPLIED PRIME COAT AND A SHOP APPLIED INTERMEDIATE COAT OF EPOXY MASTIC ALUMINUM MANUFACTURED BY SHERWIN WILLIAMS OR AN APPROVED EQUAL. AS LISTED IN THE NYSDOT MATERIALS AND EQUIPMENT APPROVED LIST FOR STRUCTURAL STEEL PAINT, CLASS 2
- IN ACCORDANCE WITH NYC DOT SPECIFICATIONS A URETHANE TOP COAT THAT IS ALSO MANUFACTURED BY SHERWIN WILLIAMS OR AN APPROVED EQUAL SHALL BE APPLIED IN THE FIELD BY OTHERS.

BENDALL INDUSTRIES, INC. 4459 BRONX BLVD BRONX, NY 10470 718-325-1588V 718-325-6685F	SCALE: N.T.S. BY: LW DATE: 1-21-2013
	STEEL FACED CURB SIDEWALK PEDESTRIAN RAMPS REF. NYC DOT DWG H-1011 7-1-2010